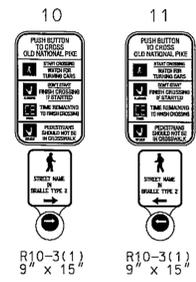


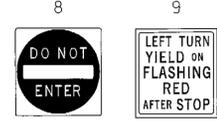


U.S. 40 IS ASSUMED TO RUN IN AN EAST-WEST DIRECTION

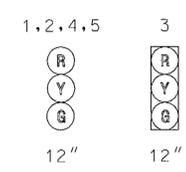
PROPOSED SIGNS



EXISTING SIGNS



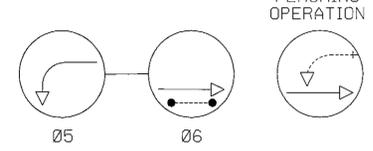
EXISTING SIGNALS



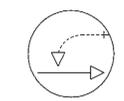
PROPOSED SIGNALS



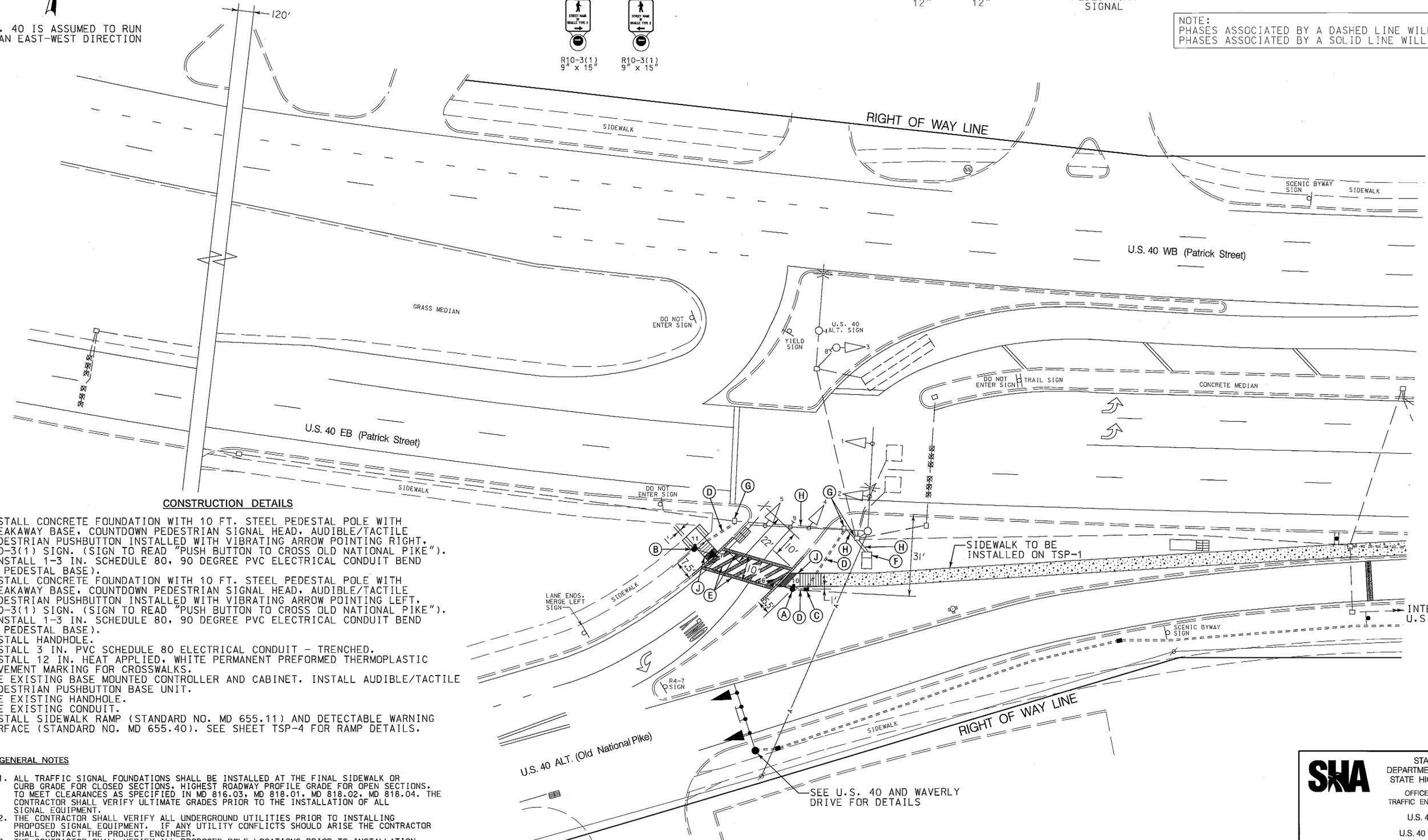
NEMA PHASING



FLASHING OPERATION



NOTE: PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY. PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.



CONSTRUCTION DETAILS

- A. INSTALL CONCRETE FOUNDATION WITH 10 FT. STEEL PEDESTAL POLE WITH BREAKAWAY BASE, COUNTDOWN PEDESTRIAN SIGNAL HEAD, AUDIBLE/TACTILE PEDESTRIAN PUSHBUTTON INSTALLED WITH VIBRATING ARROW POINTING RIGHT, R10-3(1) SIGN. (SIGN TO READ "PUSH BUTTON TO CROSS OLD NATIONAL PIKE"). (INSTALL 1-3 IN. SCHEDULE 80, 90 DEGREE PVC ELECTRICAL CONDUIT BEND IN PEDESTAL BASE).
- B. INSTALL CONCRETE FOUNDATION WITH 10 FT. STEEL PEDESTAL POLE WITH BREAKAWAY BASE, COUNTDOWN PEDESTRIAN SIGNAL HEAD, AUDIBLE/TACTILE PEDESTRIAN PUSHBUTTON INSTALLED WITH VIBRATING ARROW POINTING LEFT, R10-3(1) SIGN. (SIGN TO READ "PUSH BUTTON TO CROSS OLD NATIONAL PIKE"). (INSTALL 1-3 IN. SCHEDULE 80, 90 DEGREE PVC ELECTRICAL CONDUIT BEND IN PEDESTAL BASE).
- C. INSTALL HANDHOLE.
- D. INSTALL 3 IN. PVC SCHEDULE 80 ELECTRICAL CONDUIT - TRENCHED.
- E. INSTALL 12 IN. HEAT APPLIED, WHITE PERMANENT PREFORMED THERMOPLASTIC PAVEMENT MARKING FOR CROSSWALKS.
- F. USE EXISTING BASE MOUNTED CONTROLLER AND CABINET. INSTALL AUDIBLE/TACTILE PEDESTRIAN PUSHBUTTON BASE UNIT.
- G. USE EXISTING HANDHOLE.
- H. USE EXISTING CONDUIT.
- J. INSTALL SIDEWALK RAMP (STANDARD NO. MD 655.11) AND DETECTABLE WARNING SURFACE (STANDARD NO. MD 655.40). SEE SHEET TSP-4 FOR RAMP DETAILS.

GENERAL NOTES

- 1. ALL TRAFFIC SIGNAL FOUNDATIONS SHALL BE INSTALLED AT THE FINAL SIDEWALK OR CURB GRADE FOR CLOSED SECTIONS, HIGHEST ROADWAY PROFILE GRADE FOR OPEN SECTIONS, TO MEET CLEARANCES AS SPECIFIED IN MD 818.03, MD 818.01, MD 818.02, MD 818.04. THE CONTRACTOR SHALL VERIFY ULTIMATE GRADES PRIOR TO THE INSTALLATION OF ALL SIGNAL EQUIPMENT.
- 2. THE CONTRACTOR SHALL VERIFY ALL UNDERGROUND UTILITIES PRIOR TO INSTALLING PROPOSED SIGNAL EQUIPMENT. IF ANY UTILITY CONFLICTS SHOULD ARISE THE CONTRACTOR SHALL CONTACT THE PROJECT ENGINEER.
- 3. THE CONTRACTOR SHALL VERIFY ALL PROPOSED POLE LOCATIONS PRIOR TO INSTALLATION.
- 4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TERMINATING ALL SIGNAL CABLES TO THE APPROPRIATE TERMINALS AND PROPERLY LABEL EACH CABLE.
- 5. PUSHBUTTONS ARE TO BE LOCATED SO THAT THEY CAN BE ACTIVATED BY A PERSON IN A WHEELCHAIR REACHING LESS THAN 18" FROM A 60" x 60" LEVEL LANDING AREA WITH A CROSS SLOPE OF LESS THAN OR EQUAL TO 2%.
- 6. THE 10' SEPARATION BETWEEN PUSHBUTTONS IS TO BE MEASURED FROM FACE OF PUSHBUTTON TO FACE OF PUSHBUTTON, NOT CENTER TO CENTER OF POLE.
- 7. PUSHBUTTON ARROWS ARE TO BE PARALLEL TO THE CROSSING FOR WHICH THEY ARE INTENDED.
- 8. LOCATION OF ACCESSIBLE PEDESTRIAN SIGNAL PUSHBUTTONS MUST MEET LOCATION REQUIREMENTS OF MUTCD SEC. 4E.09 AND FIG. 4E-2 AND THE NCHRP PUBLICATION, "ACCESSIBLE PEDESTRIAN SIGNALS: GUIDE TO BEST PRACTICE". IF NOT MET, THE CONTRACTOR IS TO STOP WORK ON PUSHBUTTON LOCATIONS UNTIL A DESIGN WAIVER IS OBTAINED, APPROVED BY THE DIRECTOR, OFFICE OF TRAFFIC AND SAFETY.
- 9. VERIFY PROPOSED GEOMETRICS PRIOR TO INSTALLING SIGNAL EQUIPMENT.

TOD NO:AX142-27M  
SHA NO:FR552A57/B57  
US 40@WAVERLY DR/US 40@US 40 ALT.

**SHA** STATE OF MARYLAND  
DEPARTMENT OF TRANSPORTATION  
STATE HIGHWAY ADMINISTRATION  
OFFICE OF TRAFFIC & SAFETY  
TRAFFIC ENGINEERING DESIGN DIVISION  
U.S. 40 (Patrick Street) and  
U.S. 40 ALT. (Old National Pike)

APPROVALS		REVISIONS	
TEAM LEADER			
ASST. DIR. CHIEF			
DIVISION CHIEF			
OFFICE DIRECTOR			

SCALE 1" = 20' ADVERTISED DATE 10/10/2008 CONTRACT NO. \_\_\_\_\_

DESIGNED BY \_\_\_\_\_ COUNTY Frederick

DRAWN BY \_\_\_\_\_ LOGMILE 10004011.67

CHECKED BY \_\_\_\_\_ TIMS NO. 1760

F.A.P. NO. \_\_\_\_\_ TOD NO. \_\_\_\_\_

TS NO. 3256A DRAWING TSP-2 OF 5 SHEET NO. 2 OF 5

**WR&A**  
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